



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in **DRAFT** form. The City of Aviation Department is presenting these drafts to the public prior to public hearing to respond to public feedback. Your input is critical!



INTRODUCTION

These design standards provide a framework to achieve the following goals. Unless stated otherwise, the design standards are assumed to apply to all areas within the DEII boundary.

1. Create an attractive built environment that promotes the unique sense of place and identity of DEII, and supports opportunities for non-aeronautical, revenue generating development.
2. Define a common aesthetic for site, architectural and landscape design that results in a consistent identity across the DEII property.
3. Encourage quality developments that further the successful operation of DEII and are compatible with the surrounding visual context.

ALLOWABLE USES

DEII is zoned NR-SU, Non-Residential Sensitive Use, within the Airport Protection Overlay (APO). The following uses are permitted within this zone, and in accordance with the Double Eagle Master Plan, provided such uses are conducted in a compatible and harmonious matter to airport use.

1. Aeronautical uses
2. Non-aeronautical uses including
 - Commercial
 - Educational
 - Film studio
 - Hotel
 - Light industrial
 - Renewable energy generation

3. Wireless Telecommunications Facilities pursuant to approval by the FAA and the City Aviation Department and provided they comply with concealment requirements of the Integrated Development Ordinance (IDO) Section 14-16-4-3(E)(12)(a).
4. Accessory uses include fuel storage, including petroleum-based fuel.

SITE DESIGN

Site design should be utilized to create a functional environment for employees and visitors. Buildings on site should be designed in a manner that reinforces the identity of DEII. Future buildings shall be laid out to work with overall airport circulation corridors, with flight pathways and approaches, and be respectful of adjacent/proximate open spaces.

1. All development shall comply with the City of Albuquerque's IDO and the Development Process Manual (DPM), except where FAA regulations supersede or is stated otherwise within these design standards
2. Developments shall not be located within 150 feet of any property lines that abut Major Public Open Space. See more detail about buffering in the screening section.
3. Cross access easements between adjoining tracts are encouraged.
4. Loading areas shall follow the general building and site design regulations for Non-Residential Uses of the City's IDO.
5. Non-aeronautical developments shall provide at least one (1) minimum 250 square foot outdoor patio area for buildings over 30,000 square feet gross floor area. Development tracts with multiple buildings can aggregate requirements into one large gathering space. Patio areas should provide shade that covers a minimum 25% of the area, to the maximum extent feasible.
6. Perimeter walls, if provided, shall comply with Section 14-16-5-7 of the City's IDO, except:
 - a. Chain-link fencing is allowed for airport security fencing purposes and for the use of view fencing within the aviation area.
 - b. The use of razor or barbed wire, wood fencing, or plastic vinyl fencing is not permitted unless required by FAA

7. Minimum dimensional standards are summarized below:

DEII Key Standards Summary Table	
Standards	Requirement
Setbacks	
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd	150 feet
Setback from Double Eagle Rd	50 feet
Setback from Adjoining Properties	25 feet
Property Line Setback	25 feet
Taxilane Setback	35 feet
Building Height	
All Building Height	85 feet maximum, provided they comply with FAA FAR Part 77
Parking Setbacks	
Setback from Major Public Open Space	100 feet*
Setback from Atrisco Vista Blvd and Double Eagle Rd	25 feet*
Setback from Property Line or Lease Line	10 feet, except when a shared parking lot is provided
Lighting	
Streetlights, not including public ROW	30 feet
Parking Area Lights	30 feet
Pedestrian Lighting	16 feet
Any lighting on tracts adjacent to Major Public Open Space	20 feet

*Any lighting on tracts adjacent to Major Public Open Space



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in **DRAFT** form. The City of Aviation Department is presenting these drafts to the public prior to public hearing to respond to public feedback. Your input is critical!



BUILDING HEIGHT

1. Maximum allowable building height **shall not exceed 85'**.
2. Building heights and structures shall comply with FAA regulations. The FAA limits building and signage heights to below Federal Aviation Regulation (FAR) Part 77, Objects Affecting Navigable Airspace, surfaces including a 7:1 (horizontal to vertical from a distance 250 ft. from runway centerline) Transitional Surface and the Horizontal Surface Have the regulations in the reference

folder. located 150 ft. above airport elevation. Additionally, FAA FAR Part 77 states that any structure penetrating the plane created by a 100:1 sloped surface beginning at the nearest point, or the nearest runway must be airspaced by FAA in accordance with Part 77. Notification is required by completing and submitting SF 7460, Notice of Proposed Construction of Alteration, to the FAA for review and airspacing.

SETBACKS

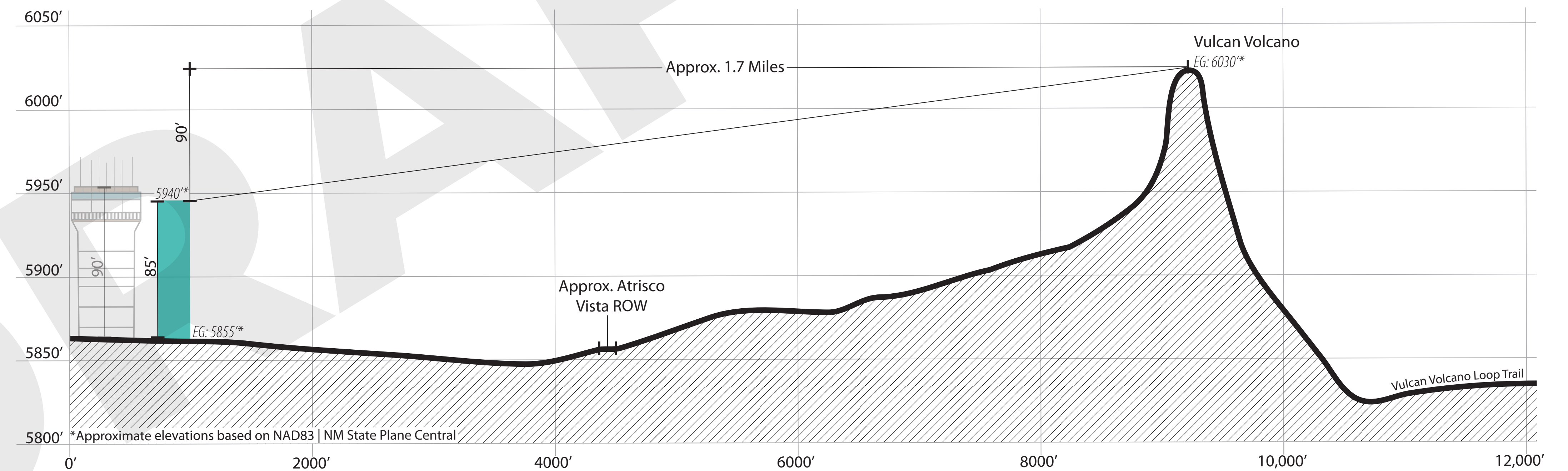
The use of building and parking area setbacks is required to provide space for the creation of a visually attractive entrance streetscape, provide a safe and compatible environment for aircraft, service vehicles, and pedestrians throughout DEII, and provide buffers to adjacent public open space. Elements required within these setbacks will be pedestrian walkways, screening devices, and landscape improvements.

1. Minimum building setbacks shall be as follows:

Setback Standards	Requirement
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd	150 feet
Setback from Double Eagle Rd	50 feet
Setback from Adjoining Properties	25 feet
Property Line Setback	25 feet
Taxilane Setback	35 feet

2. Buildings shall not be located closer to the airfield than the Building Restriction Line as shown on the "Airport Layout of the Plan" of the DEII Master Plan.
3. Buildings shall maintain a clear line of sight from the air traffic control tower controller positions to airfield pavements on the airfield.

VIEWSHED FROM TOP OF VULCAN VOLCANO



Conceptual Cross Section of Vulcan Volcano at Petroglyph National Monument



Panoramic view of Petroglyph National Monument Spatter Cones East of Atrisco Vista Blvd



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

BUILDING DESIGN

1. The scale, proportion, and composition of fenestration of facades of non-utilitarian buildings shall be designed to create visual interest.
2. Primary facades of non-aeronautical buildings greater than 100 feet in length should comply with the building design standards of Section 14-16-5-11 of the City's IDO where feasible.
3. Primary and Accessory buildings must comply with Major Public Open Space setback requirements.
4. The major entry to each non-aeronautical building should be clearly defined, face the primary circulation corridors, and connect to pedestrian pathways to the maximum extent feasible.
5. Columns, corner articulation, overhangs, awnings, gutters and scuppers, breezeways, and soffits shall be carefully dimensioned and detailed to provide a human scale and visual interest.
6. Special attention shall be given to the articulation of aircraft hangar facilities through use of color, materials, or building offsets. Metal structures are acceptable, provided they visually tie to other building elements, such as the front office, storage areas, etc.
7. Any glass or metal elements must comply with FAA standards addressing glint and glare.
8. Temporary buildings are subject to the temporary structure standards of Section 14-16-5-4-3 of the City's IDO and shall be allowed for a maximum period of one (1) year. After one-year, temporary structures must be removed or improved to meet DEII standards.

MATERIALS AND COLORS

1. Acceptable colors of exterior building materials for all development on DEII should take their cues from the native New Mexican landscape palette, including but not limited to: Sand, Terracotta, Blues and Teals, and Sage Green. Due to the proximity to the Major Public Open Space, subdued colors are preferred.
2. Buildings sited within 250' of the property line of Major Public Open Space shall:
 - a. Utilize colors that blend with the surrounding natural

environment and generally include yellow ochres, browns, dull reds, and grey greens.

- b. Limit the colors of exterior surfaces of structures such as mechanical devices, roof vents, and screening materials to those with light reflective value (LRV) rating between 20 percent and 50 percent.
3. The use of accent colors and materials is encouraged to bring out detailing to better articulate or give scale to a building, including glazed tile, wood trim, tile roofing, paint, metal, etc.
 4. Special consideration shall be given to roof structures. The use of contrasting colors between roofs and walls is encouraged to help differentiate the planes of building masses.
 5. Prohibited building materials include the following:
 - a. Plastic or vinyl building panels, awnings, or canopies.
 - b. Exposed, untreated precision block or wood.
 - c. Highly reflective surfaces
 - d. Materials with high maintenance requirements
 - e. Buildings that are predominately white in color

BUILDING PRECEDENTS



Flying High Hangars

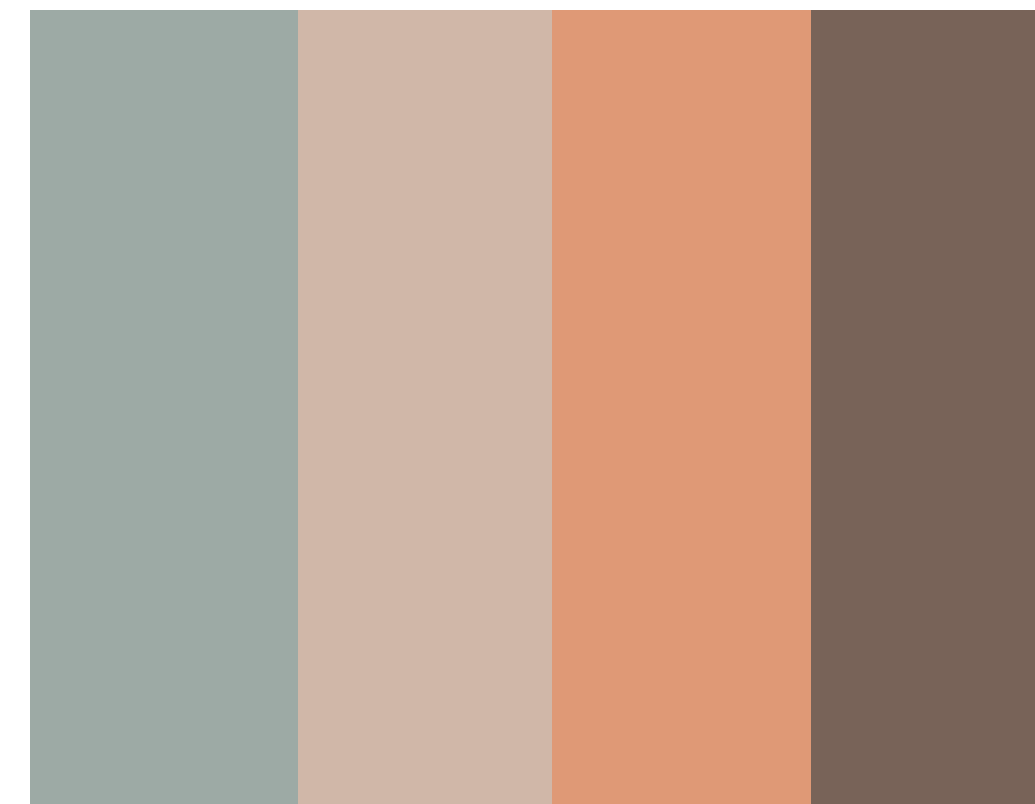


One Presidential Plaza



Altura Office Building

General Colors adjacent to Petroglyph National Monument



General Colors of New Mexico Natural Landscape



Pathway Office Park



Red Rock Office Building



Sennheiser Office Warehouse



Amazon Warehouse



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in draft form. The City of Aviation Department is presenting these draft to the public prior to public hearing to be able to respond to public feedback. Your input is critical!



ACCESS AND CIRCULATION

DEII is uniquely situated adjacent to a designated Major Public Open Space (Petroglyph National Monument) while operating as an airport servicing many types of vehicular traffic. Atrisco Vista along the east side of the airport is planned as a regional transportation corridor, providing access to and from Interstate 40. To provide safe and appropriate transportation infrastructure the following guidelines shall govern.

1. Access off Atrisco Vista Blvd shall be coordinated with NMDOT and other applicable agencies.
2. Sites should be designed to minimize conflicts between automobiles, trucks, bikes, and pedestrians to create an organized system of entrances, driveways, parking lots, and delivery areas.
3. DEII shall designate specific areas for the pickup and drop-off of passengers and guests utilizing rideshares, taxi, and guest transportation within the aviation areas.
4. Passenger and guest transport vehicles, including rideshare, taxis, shuttles, etc., shall proceed to the designated drop-off and pickup location at DEII.
5. Passenger and guest transport vehicles should not enter the airfield gates or travel elsewhere without explicit permission and/or escort by DEII personnel.
6. DEII shall designate delivery routes for the aviation areas of the property. All tenants must submit commercial delivery routes to DEII for approval.
7. A minimum six (6) foot clear pedestrian pathway shall be provided from each building to the internal circulation system and to adjacent roadways.

8. Commercial delivery trucks must enter DEII within the secured air operations area, follow defined DEII delivery routes, and must be escorted at all times; all unescorted commercial delivery traffic is prohibited.

TAXILANES

Aircraft circulation throughout DEII shall be developed with minimum interaction with vehicular traffic. Taxilane standards are based on an average tailing speed of at least 20 mph and wingspan and wingtip clearance. Taxilane access must have a minimum right-of-way width of 150 feet with a minimum paved surface width of 50 feet.

1. Taxilane shoulders must be either stabilized or paved to reduce the possibility of blast erosion and engine ingestion problems.
2. Minimum separation between centerline of taxilane to structure (parked aircraft, structure, etc.) is 0.70 times the wingspan of the largest airplane, plus 10 feet. Setback standards have been designed to accommodate all Type I and II aircraft.

Taxilane Standards	Requirement
Airplane Design Group	II
Taxilane Width	35 feet
Taxilane Object Free Area Width	124 feet
Taxilane Shoulder Width	15 feet
Wingspan (w) for Airplane Design Group	49 feet ≤ w 79 feet

4. DEII facilities located adjacent to the airport taxilane system or service corridors are assumed to support aviation related activities, which may require outside storage of aircraft as well as airfield maintenance equipment.

PARKING

Parking areas should be designed to mitigate heat/glare through the provision of landscaping, minimize the visual impact of parking areas, and provide accessible and safe circulation within and adjacent to the parking areas. Off-

street parking areas, including minimum off-street parking requirements, space size, aisle widths, pedestrian pathways, and screening, shall comply with Section 14-16-5-5 of the City's IDO and the DPM.

1. Off-street parking areas, including minimum off-street parking requirements, space size, aisle widths, pedestrian pathways, and screening, shall comply with Section 14-16-5-5 of the City's IDO and the DPM.
 - a. Film Studio uses will utilize requirements for Light Industrial uses of the City's IDO.
2. The City Aviation Department shall approve the design of all parking areas on DEII property.
3. Unstriped or pervious parking areas are permissible around buildings of aeronautical uses for temporary airplane or vehicle parking.
4. Car/vanpool preferred parking shall be provided near building entrances and shall be clearly demarcated, provided they do not conflict with FAA safety regulations.
5. Parking lots should employ Low Impact Design (LID) techniques and direct surface runoff to landscaped water harvesting areas.
6. Alternate paving materials are permitted for both vehicular and pedestrian zones.
7. Bicycle parking at a ratio of one (1) bicycle rack per 50 vehicular parking spaces shall be provided to encourage non-vehicle use. Bicycle racks should be conveniently located near building entrances.
8. Bicycle racks or lockers shall be installed per the City's DPM.

PERVIOUS SURFACE PRECEDENT



Pervious Surface Materials on Parking Lot

RIDESHARE PRECEDENT



Rideshare Pick-up with covered seating area



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in draft form. The City of Aviation Department is presenting these draft to the public prior to public hearing to be able to respond to public feedback. Your input is critical!



LIGHTING

Lighting should enhance the safety, security, and visual aesthetics of DEII. The primary objective of site lighting shall be to maximize public safety without impacting the FAA requirements of the site or generating unnecessary glare or reflection to adjacent properties or Major Public Open Spaces.

1. All lighting shall generally comply with the outdoor lighting standards of Section 14-16-5-8 of the City's IDO
2. Specific lighting fixtures styles shall be established by / reviewed by DEII and shall be consistent with other projects within DEII property. Lighting fixtures shall be fully shielded horizontal lamps to eliminate light trespass beyond the property line and no light source shall be visible from the site perimeter except where FAA regulations and standards may vary.
3. The maximum height standards of light fixtures shall not exceed the following:

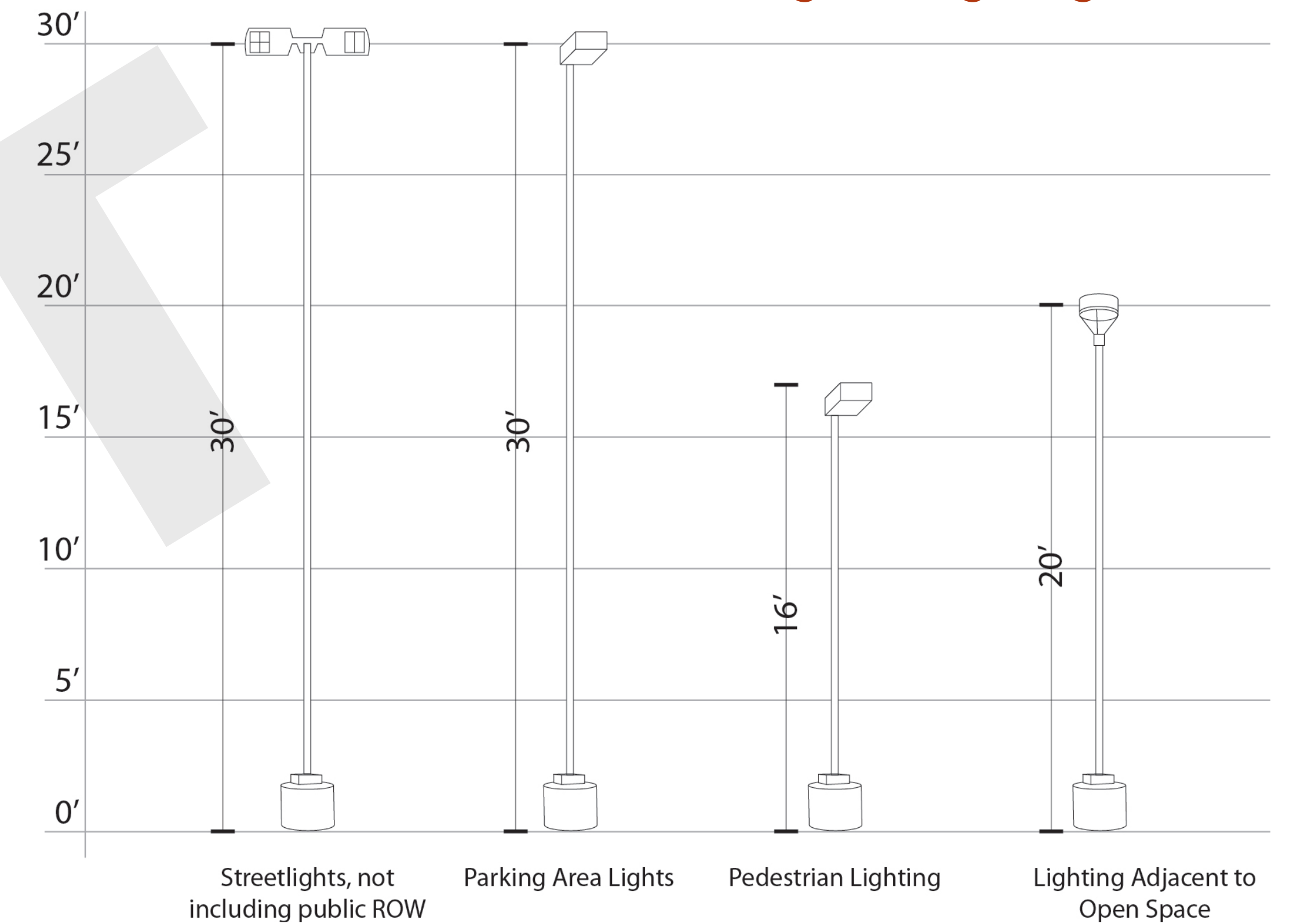
Lighting Standards Table	Requirement
Streetlights, not including public ROW	30 feet
Parking Area Lights	30 feet
Pedestrian Lighting	16 feet
Any lighting on tracts adjacent to Major Public Open Space	20 feet

SIGNAGE

Signage should create a sense of arrival to DEII and ensure proper wayfinding for visitors, employees, and deliveries.

1. All signage shall follow signage regulations of Section 14-16-5-12 of the City's IDO unless otherwise noted herein.
2. Signage design and locations shall be approved at the discretion of DEII. Monument signage shall be coordinated to have the same appearance as established precedents on DEII property.
3. Future development of tracts is limited to two (2) monument signs per the City's IDO size requirements.
4. Directional signs to direct visitors and delivery services may be provided at the discretion of DEII.
5. Prohibited signs include the following:
 - a. Banners, pennants, ribbons, streamers.
 - b. Brashly colored signs with moving or flashing lights.
 - c. Animated, illuminated, flashing, or electronic signs.
 - d. Portable signs.
 - e. Off-premises signs referring to a business or merchant not conducting business on the premise where the sign is displayed.
6. Temporary banners, pennants, or ribbons may be permitted for special, time-limited events at the discretion of DEII. Temporary signage must be removed by the agreed-upon timeline and must comply with FAA requirements.
7. No sign may overhang a right-of-way or property line, intrude upon any architectural or decorative features, or interfere with clear sight requirements of the City's DPM.
8. Refer to the PNM Electric Service Guide at www.pnm.com for specifications.
9. When an above-ground backflow prevention device is required by the City of Albuquerque, the heated enclosure shall be constructed of materials compatible with the architectural materials used as the main elements of the building. If prefabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.

Maximum Height of Lighting Fixtures



SIGNAGE PRECEDENT



Signage from Aerospace Technology Park Entrance



Signage from Double Eagle Road Entrance



Signage from Tohono Chul Park Entrance



Tenant Signage Example



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

LANDSCAPING

Site landscape shall serve to enhance the visual aesthetic of the campus and aid in reinforcing the street presence and pedestrian experience without interfering with the site's primary aviation function. The primary focus for site landscape should emphasize visitor and employee centric spaces of the campus including the main entry, primary buildings entrances, pedestrian pathways, and patios or gathering spaces. The intent is to provide year-round color and interest and reflect the natural environment of New Mexico landscape.

1. Landscaping plans should generally comply with the Section 14-16-5-6 of the City's IDO. On-site uses may utilize Alternative Landscape Plans to ensure site compliance with FAA regulations or optimize operations.
 - a. Aeronautical uses shall not be required to comply with the City's IDO landscaping to ensure compliance with FAA requirements.
 - b. Non-aeronautical uses may request relief from standards that interfere with or compromise their functionality, for example parking lot landscape requirements interfering with the functionality of parking lots within film studios.
 - c. Total net site and coverage requirements should be calculated for the entire DEII site, provided that only the developed portions of the site are used for calculations.
2. Future development tracts within DEII property shall be maintained in their natural condition until developed. If these areas are disturbed, they shall be re-vegetated with native seed mix appropriate for this region.
3. Development tracts adjacent to the Major Public Open Space shall provide a minimum six (6) foot landscape strip that varies in width to avoid the appearance of a hard, straight line. Plant coverages and selections within this buffer shall provide a visual barrier, with the exception of areas within the Airport Runway Protection Zone of the City's IDO.
4. All plant material shall be maintained by the tenant in a living, attractive condition. All areas shall be maintained and be free of weeds.

5. Due to the aviation nature of the site, trees should only be provided along the streetscape of Double Eagle Rd, within patio or gathering spaces, and in landscape buffers adjacent to Major Public Open Space. Tree selections and locations shall be coordinated with DEII to ensure they do not conflict with aviation operations.
6. A specific landscape palette may require review by the City Aviation Department's USDA Wildlife Biologist and conform to current FAA regulations, including those restrictions of specific plants and trees that attract animals that conflict with aviation operations.
 - a. The use of native or regionally adapted plant materials that will thrive in local conditions with less fertilizer, water, and maintenance is encouraged.
 - b. Fruit and nut trees or shrubs, which may attract wildlife, shall not be permitted on DEII.
 - c. High water use turf is restricted to patio or public gathering areas within DEII.
7. Landscape design shall incorporate active and passive water harvesting techniques to reduce potable water use for irrigation where possible.
8. All landscape areas shall be stabilized with mulches or plant material to keep soils from washing or blowing away. Gravel mulch, bark mulch, and similar materials are acceptable as a top dressing for landscape areas.
9. An automatic underground irrigation system is required to support all site landscaping. The system shall be designed to avoid over-spraying onto pedestrian paths, buildings, fences, etc.
10. Landscaping shall not interfere with clear sight requirements of the City's DPM. Trees and shrubs between 3 and 8 feet in height (as measured from the gutter pan) shall not be acceptable in this area.

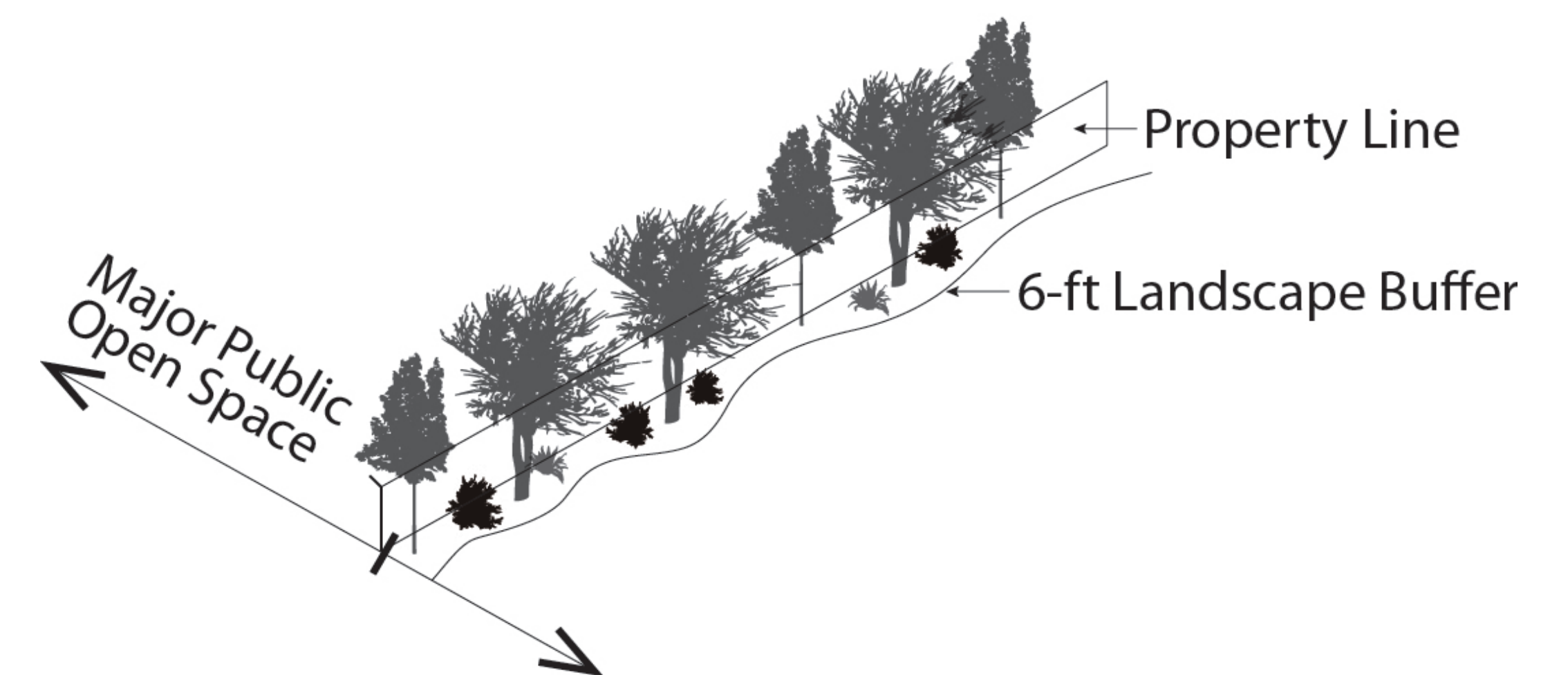
SCREENING

Screening of less attractive areas such as loading, service, and/or mechanical area, including but not limited to exterior refuse facilities, mechanical equipment, storage yards, and loading docks, should be accomplished using walls and fences or landscape buffers that are designed to be an integral part of DEII's overall aesthetic. All such functions, except for airplanes viewed from taxilanes, should be

designed in an aesthetically pleasing manner, or screened from Atrisco Vista Blvd, and from any designated Major Public Open Spaces.

1. Parking areas adjacent to Atrisco Vista Blvd, other public rights-of-ways, or within 100 feet of Major Public Open Space shall be screened through measures including not limited to perimeter walls, landscaping, or the use of buildings.
2. Loading, service, and refuse areas shall be integrated into the building design if possible or be located so not to be visible from Atrisco Vista Blvd, other public streets, or the Major Public Open Space. If not feasible, they shall be screened from view by an opaque decorative wall or fence at least six (6) feet tall that is compatible with materials and design of the building with which it is associated.
3. Mechanical equipment shall be screened from Atrisco Vista Blvd, other public right-of-way, and the Major Public Open Space pursuant to Section 14-16-5-5-6 of the City's IDO and be compatible with materials and design of the building with which it is associated.
4. Passenger loading areas do not require screening.

Landscape Buffer to Major Open Space





DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

SUSTAINABILITY

Green architectural design is encouraged, with a particular emphasis on creating facilities that do not rely on carbon-producing sources of energy. DEII promotes the use of sustainable design principles, environmentally responsible building concepts, and lower embodied carbon building practices. DEII supports the use of solar photovoltaic, wind turbines (if compliant with glint and glare studies), ground-source heat pumps, and other alternative energy production facilities on site.

1. Stormwater control measures shall be designed to manage the first flush and control runoff generated by contributing impervious surfaces.
2. Buildings shall be oriented to take advantage of heat gain in the winter where possible while coordinating with shading strategies to inhibit solar gain in the summer.
3. Roofs shall drain water to areas which are landscaped appropriately for such run-off.
4. Permeable paving may be used on site within parking areas, or pedestrian areas provided it does not compromise the ability to carry the applicable loads required for parking, vehicular activity including freight trucks, or trash hauling vehicles.
5. Parking islands shall allow flow of water as required to maintain proper drainage of the site.
6. Grasses and other ground vegetation should be placed near project edges to help filter and slow runoff as it exits and enters the site.

UTILITIES

Utility systems should ensure the minimum expectations for the design of new or renovated service connections at DEII. The systems shall enhance the safety measures of FAA and the aesthetic of DEII.

1. Developers shall contact PNM's New Service Delivery Department to coordinate electric service and options for the location of electric service connection. Any existing or proposed public utility distribution easements are to be indicated on subsequent Site Plan for Building Permit utility sheets. PNM's standard for public utility distribution easements is 10 feet in width to ensure adequate, safe clearances.
2. All electric distribution lines shall be placed underground.
3. Transformers, utility pads, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way. Screening of transformers, utility boxes and other utility structures shall not impede access to the equipment and shall provide for safe maintenance and repair by utility workers.
4. Transformers, utility pads, and ground-mounted equipment screening shall allow for access to utility facilities. All screening and vegetation surrounding ground mounted transformers and utility pads are to allow 10 feet of clearance in front of the equipment door and 5-6 feet of clearance on the remaining three sides for safe operation, maintenance, and repair purposes.

SUSTAINABILITY PRECEDENTS



Bicycle Technologies International Warehouse



Existing Stormwater Control at NM State Police Hangar



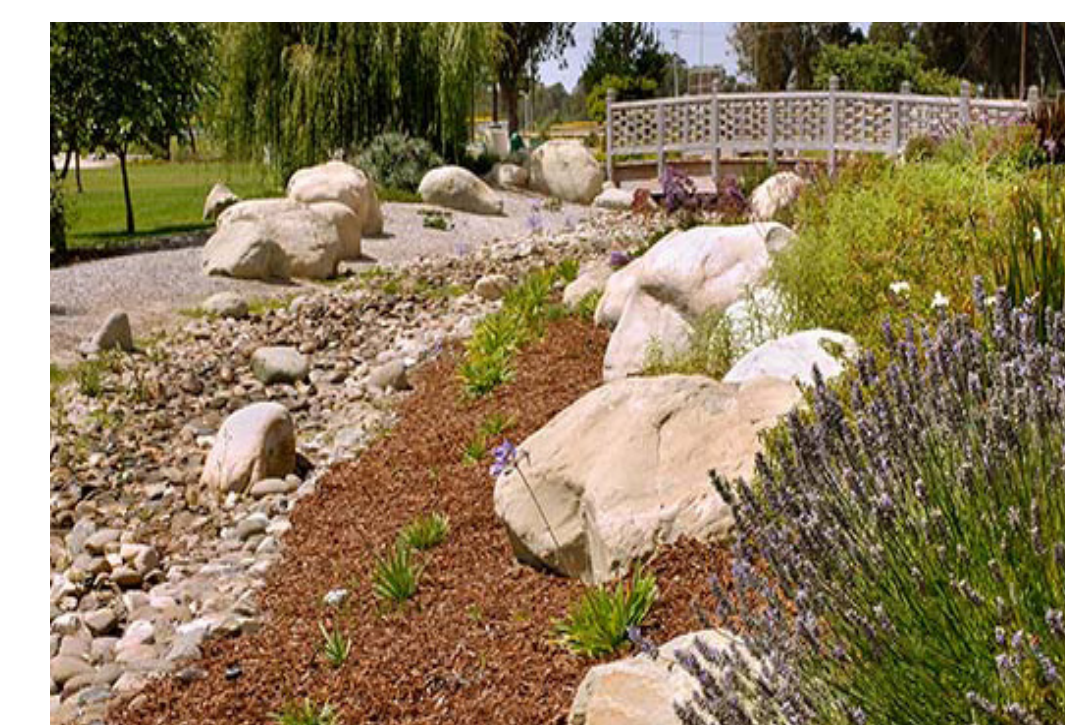
Existing Stormwater Control from an Executive Hangar



Landscape Improvements within Drainage Areas



Landscape Improvements within Drainage Areas



Ground Vegetation within Stormwater Control Area



Parking Island Stormwater Control Area